

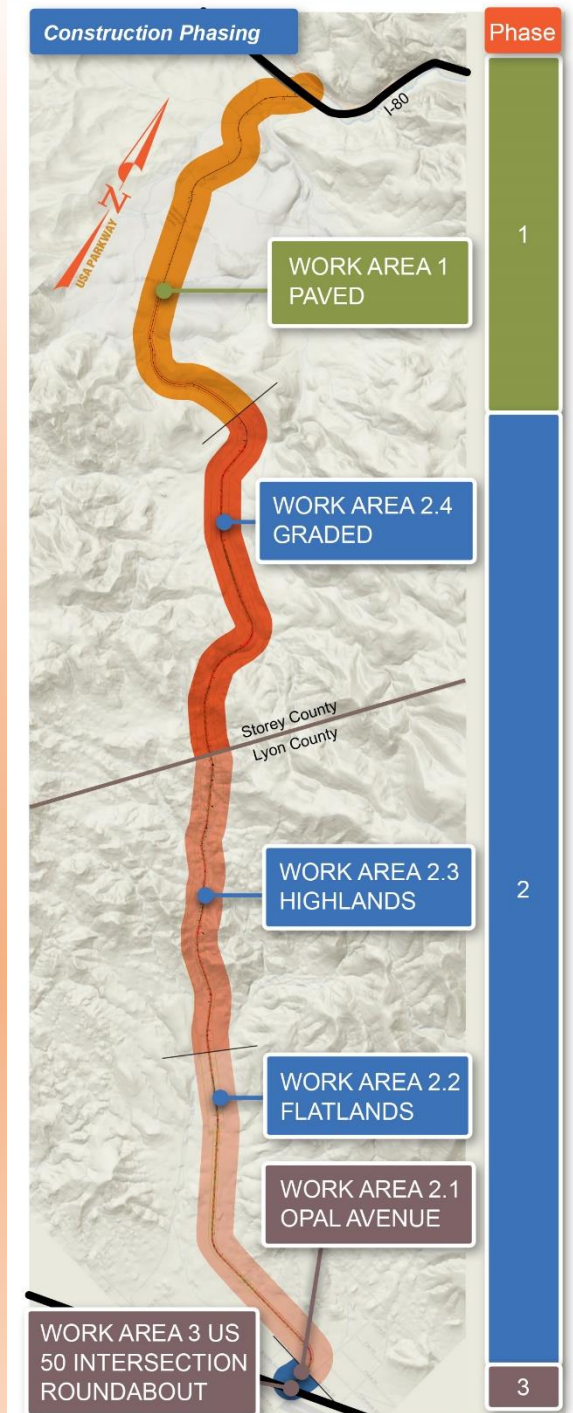
A large yellow Caterpillar dump truck is shown from a low angle, parked on a dirt road. The truck's bed is raised, and it is loaded with dark rocks. The background features a snowy mountain slope under a blue sky with white clouds. The text "Ames Construction" is overlaid in a large, red, outlined font across the middle of the image.

Ames Construction

USA Parkway

Project Overview

- 6 miles of Safety improvements on previously constructed USA Parkway
- 12.8 miles of new 4 lane highway with median
 - 4 miles on previously graded portion that did not meet NDOT Road Design standards
 - 4 miles through rugged topography
 - 4.8 miles through BLM property
- New intersection at US50 (Opal Ave)





Proposal



- RFP July 31, 2015
- Ames Prime Contractor
- Principle Design: Horrocks Engineering
 - CA Group – Supplementary Design Consultant
 - Terracon – Geotechnical
- Quality Control/Quality Assurance: Raba Kistner
- Proposal submission – October 19, 2015
- Notice of Lower Bidder – November 13, 2015



- \$75.9M Original Contract
- \$81.4M including Change Orders through March 17, 2017
- \$55.5M invoiced through April 29, 2017



Critical Dates

- NTP1 – January 12, 2016
- WA1 RFC – June 16, 2016
- WA2.4 Rough Grade RFC – June 21, 2016
- WA2 RFC – July 22, 2016
- WA3 RFC – September 30, 2016
- WA3 Access to Work Area – January 2017
- Substantial Completion – September 8, 2017

Work Area 1 – Safety Improvements

- Existing road (6 miles) up to AASHTO Safety Standards
- Obstructions in the clear zone



Shoulder Improvements



- Culvert extensions & Drop Inlet installation
- Safety End Section installation
- Minor Grading
- 10,000 lf of guardrail installation



Landscaping



- Steel Edging & Rock Mulch installation
- Boulder groupings
- Bare ground stabilization: rock mulch & hydroseeding
- Trees & Shrubs

Work Area 2.4 – Previously Graded Road





Work Area 2.3 – Highlands

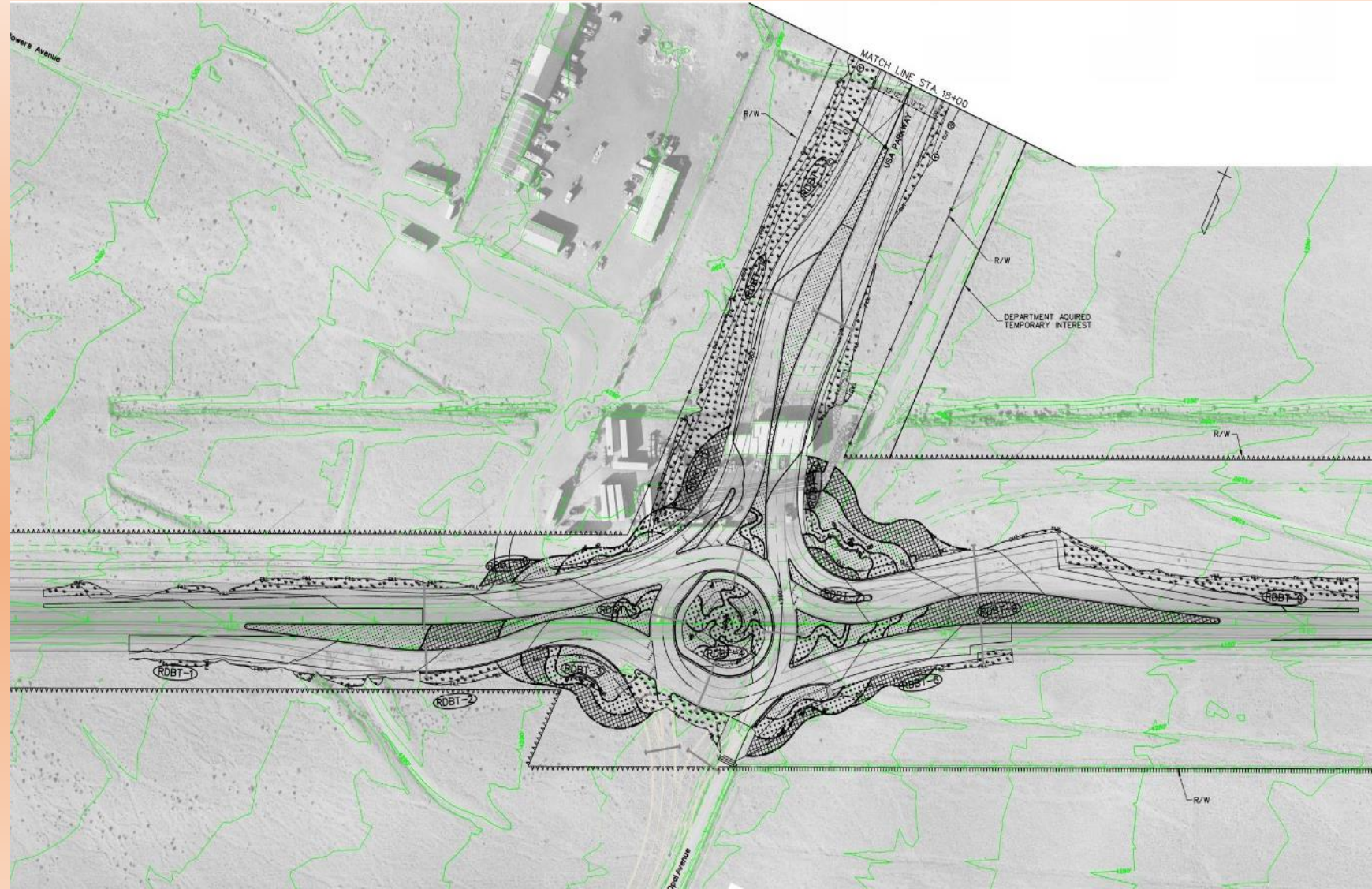


Work Area 2.2 – BLM

- Fairly flat topography
- Only $\approx 300,000$ cy of earthwork
- No rock cuts
- 2nd Wildlife Crossing

Work Area 3 – US50 Intersection

- Roundabout Intersection
- Drainage Improvements
- MOT / Phasing



Earthwork

- 2,250,000 CY Fill
- 2,400,000 CY Cut
(Includes material to crusher)
- Average swell 1.2



Average Quantity Placed per shift – 6,500 to 8,500 cy

- D10 – 7075 hrs
- D11 – 1705hrs
- 777 – 5909 hrs
- 992 – 2252 hrs

Drill and Blast

- 700,000 CY Blasted
- 4 drills
- $\approx 6,800$ MH



Underground Installation

- Box Culverts: 1,731'
- Concrete Culvert: 12,759'
- 48 Drop Inlets
- Steel Casing for future utilities – 6 locations
- Change Order for ITS trunk line from US50 to beginning of existing pavement



Structures

- Two Wildlife Undercrossings
- Headwalls on 7 Box Culverts with Wooden Peg color
- 1,500 CY of Concrete
- 5,000 lf of concrete barrier





Demolition

- Removal of three buildings in Silver Springs to make way for USA Parkway
- Asbestos tile found in two of the buildings



Site Balance

- 490,000 TN Aggregate Base
- 82,500 TN Class 150 Riprap
- 40,000 TN Riprap Bedding
- 70,000 TN Sub Grade Material
- 12,000 TN Rock Mulch



Environmental

- 8.5 miles of Rip Rap lined ditch
- 130,000 cy Rip Rap





Erosion Control

- January 2017 2nd wettest month ever in Reno (1916)
- 10.2" of precipitation in Winter 2016-2017
- Average precipitation Dec to Feb = 3.08"





Subcontractors



- Subcontract work – 42%
- 240,000 tn Dense Grade
- 544,000 sy Open Grade (Paving Footprint)
- Guardrail & Cable Barrier
- 15,250 manhours to date





- Multiple landscape Subs
- Concrete sub for barrier and curb
- Maintenance of Traffic
- ITS Change Order – Par Electric



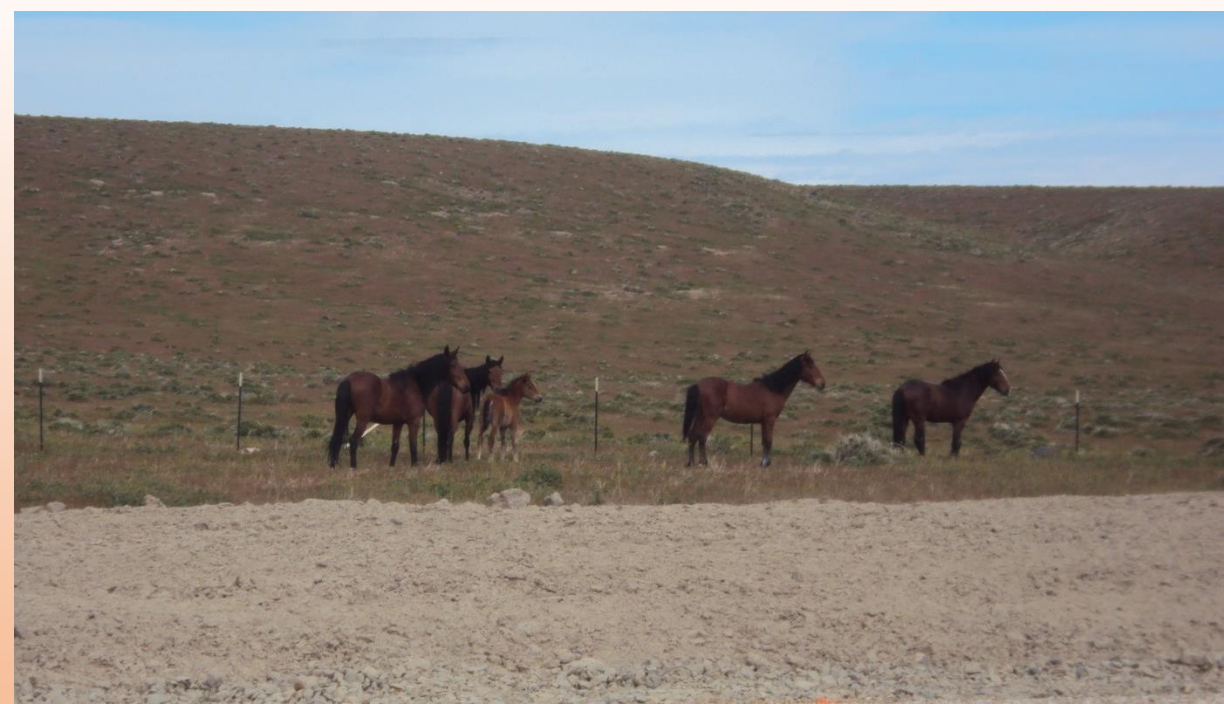
Project Sculptures

- TRIC Sculpture
- 2D Steel Emigrants Sculptures at US50 intersection



Project Challenges

- Working alongside wildlife
- Producing Aggregate Base on site
- Weather



**149,000 MH through April 29,
2017 w/no recordables or
loss time**